



**PLANNING RATIONALE AND DESIGN  
BRIEF**

450 McNeely Avenue - Parcel E, Phase 1  
Commercial Development

December 19, 2023

Prepared for:  
Calloway REIT (Carleton) Inc.

Prepared by:  
Stantec Consulting Ltd.

Project Number:  
160401912

## Planning Rationale and Design Brief

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## Planning Rationale and Design Brief

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## Table of Contents

<b>EXECUTIVE SUMMARY .....</b>	<b>ii</b>
<b>1 CONTEXT ANALYSIS .....</b>	<b>1</b>
1.1 Surrounding Context.....	1
1.2 Site Context .....	2
<b>2 PROPOSED DEVELOPMENT AND DESIGN BRIEF .....</b>	<b>5</b>
<b>3 POLICY REVIEW AND JUSTIFICATION .....</b>	<b>9</b>
3.1 Provincial Policy Statement 2020.....	9
3.2 Lanark County Sustainable Communities Official Plan (2012) .....	11
3.3 Town of Carleton Place Official Plan (2013) .....	11
<b>4 DEVELOPMENT PERMIT BY-LAW 15-2015, AS AMENDED.....</b>	<b>16</b>
4.1 Amendment Rationale .....	22
<b>5 SUPPORTING DOCUMENTATION .....</b>	<b>22</b>
<b>6 CONCLUSION.....</b>	<b>23</b>

### LIST OF TABLES

Table 1. Development Standards Compliance Review .....	17
Table 2. Review of the Highway Commercial Built Form Design Criteria.....	20
Table 3. Rationale for Proposed Amendments to the DPB .....	22

### LIST OF FIGURES

Figure 1. Surrounding context (Town of Carleton Place).....	1
Figure 2. Site context (Town of Carleton Place) .....	2
Figure 3. Streetview context (Google Streetview) .....	3
Figure 4. Survey Plan 27R-9045 (Stantec Geomatics Ltd.).....	4
Figure 5. Topographic Sketch Certified December 7, 2023 (Stantec Geomatics Ltd.).....	4
Figure 6. Site Plan Excerpt (Petroff Partnership Architects).....	5
Figure 7. Landscape Plan Excerpt (Levstek Consultants Inc.).....	6
Figure 8. Building Elevations (LLA Architecture+) .....	7
Figure 9. Building Elevations - Finish and Material Schedule (LLA Architecture+) .....	8
Figure 10. Town of Carleton Place Official Plan Schedule A.....	12
Figure 11. Town of Carleton Place Development Permit Schedule A .....	17



# Executive Summary

Stantec Consulting Ltd. (“Stantec”) is retained by Calloway REIT (Carleton) Inc. (“owner”) for the purpose of preparing this planning rationale and design brief in support of a Class 3 Development Permit application (“DP3”) for a proposed commercial development on a vacant portion of their property described municipally as 450 McNeely Avenue in the Town of Carleton Place (“Town”).

The noted vacant land is located at the northeast corner of the property and is described as “Parcel E”, which represents an area of approximately 21,591 square metres. The proposed development is for a first phase that will see approximately half of the land area of Parcel E built out ( $\pm 10,562 \text{ m}^2$ ), with the balance to be developed as part of a future phase. The technical submissions prepared in support of the application relate to the Phase 1 development and have consideration for a future phase to build-out the remainder of Parcel E.

The proposal consists of a single commercial retail building (approximate gross floor area of 2,269.34  $\text{m}^2$ ), a screened loading and waste storage area, surface parking (107 spaces), bicycle parking (8 spaces), drive aisles, and a mix of hard and soft landscaping (12 trees to be planted). As part of the development a north-south pathway along the westerly edge of Phase 1 is proposed and will provide a connection between the existing shopping centre and the pathway network north of the property. Additionally, it is proposed that the existing granular multi-use pathway (“MUP”) along the east frontage of Parcel E be re-aligned and conveyed to the County of Lanark (“County”) as a condition of development.

Parkland dedication is to be satisfied as cash-in-lieu of parkland at the commercial rate of 2% of the value of the Phase 1 land area being developed. The development has been designed to conform with applicable planning policy and to generally meet the intent of applicable regulations under the Town’s Development Permit By-law, with amendments required for a minor reduction in the required parking rate for a retail store land use (114 spaces required and 107 spaces proposed) and for loading spaces (two spaces required and one proposed).

A formal pre-application consultation with the Town was held on October 5, 2023, where the proposal was presented, and initial feedback was provided by the Town. Following the pre-application consultation, the Town confirmed the application type and supporting documentation required in order to support the proposed development through the approvals process. Through follow-up consultation the Town was informed that the planned tenant for the building will require signage on the property to announce the project, and the Town confirmed that the approval of the signage, which will require relief from By-law 65-2008 (the Town’s sign by-law), can be addressed through the DP3 process.

As demonstrated through this report and in the technical material required with the application, the proposed Phase 1 development represents an appropriate use of vacant shopping centre land, as it will help to infill the property with a land use and site design that aligns with the intent of applicable planning policy and regulations. Accordingly, we recommend the application for approval.



# 1 Context Analysis

## 1.1 Surrounding Context

The property is described as 450 McNeely Avenue in the Town of Carleton Place, which consists of a shopping centre anchored by a Wal-Mart with frontage along the west side of McNeely Avenue. The north half of the property is currently vacant, and the northeast portion of these vacant lands represents Parcel E. The subject site represents the Phase 1 area of Parcel E, which is approximately half of Parcel E. The following land uses and features surround the subject site:

- North Existing stormwater management pond and pedestrian network followed by a large vacant parcel at the southwest corner of McNeely Avenue and Coleman Street.
- South Existing drive aisle that serves as the primary vehicle access to the existing shopping centre followed by two multi-tenant commercial buildings, a Wal-Mart, and surface parking.
- East Vacant portion of Parcel E to be reserved for future development followed by a MUP and McNeely Avenue. The adjacent portion of McNeely Avenue is a multi-lane thoroughfare characterized by a mix of large and smaller format commercial retail uses.
- West Vacant lands within the same property followed by a developing residential subdivision consisting of low to medium density building forms.



Figure 1. Surrounding context (Town of Carleton Place)



## 1.2 Site Context

The subject site, being the Phase 1 area, is vacant, relatively flat, and naturalized with a mix of trees and meadow. Two separate easements transect the site in a north-south orientation, with the more westerly of these associated with a municipal sanitary sewer and watermain (Instrument No. LC55208), and the other being associated with a gas main (Instrument No. RS151417). A MUP exists along the frontage of Parcel E and is to be re-aligned and conveyed to the County as part of the DP3 process. The below details provide additional context whereas Figures 2 and 3 contain aerial and streetview images of the site and Figures 4 and 5 contain extracts of legal and topographic surveys.

Legal Description	Part of Lot 16, Concession 11, Geographic Township of Beckwith, Town of Carleton Place, County of Lanark
Land Area	±118,874 m <sup>2</sup> (property); ±21,591 m <sup>2</sup> (Parcel E); ±10,640 m <sup>2</sup> (Phase 1)
Frontage	McNeely Avenue: ±483.44 m (property); ±152 m (Parcel E / Phase 1)



Figure 2. Site context (Town of Carleton Place)



**Planning Rationale and Design Brief**

1. Facing north towards the site



2. Facing northeast towards the site



3. Facing west towards the site



4. Facing south towards the site



Figure 3. Streetview context (Google Streetview)



# Planning Rationale and Design Brief

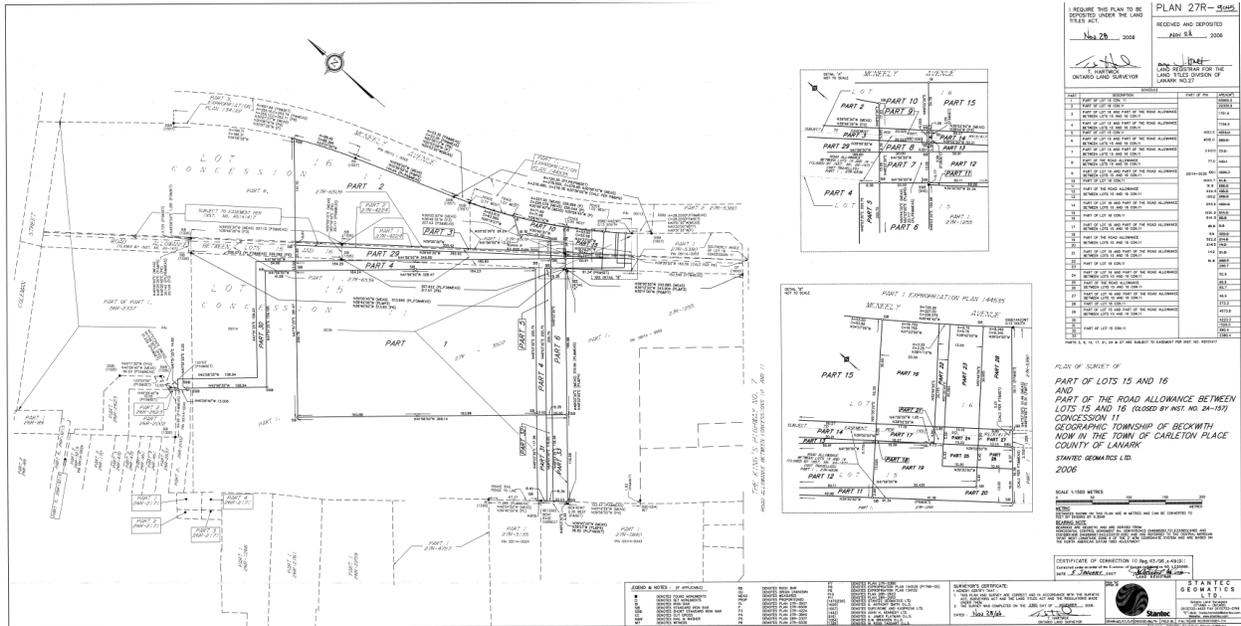


Figure 4. Survey Plan 27R-9045 (Stantec Geomatics Ltd.)

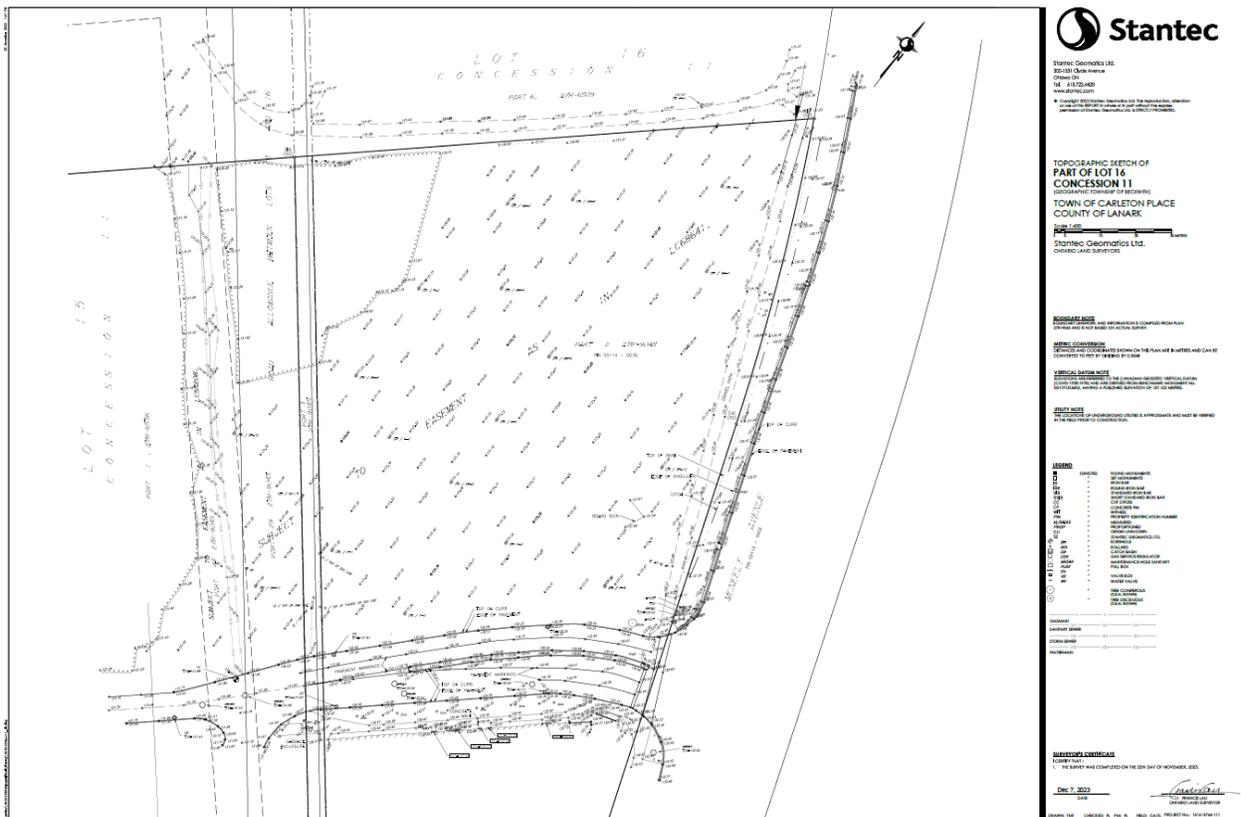
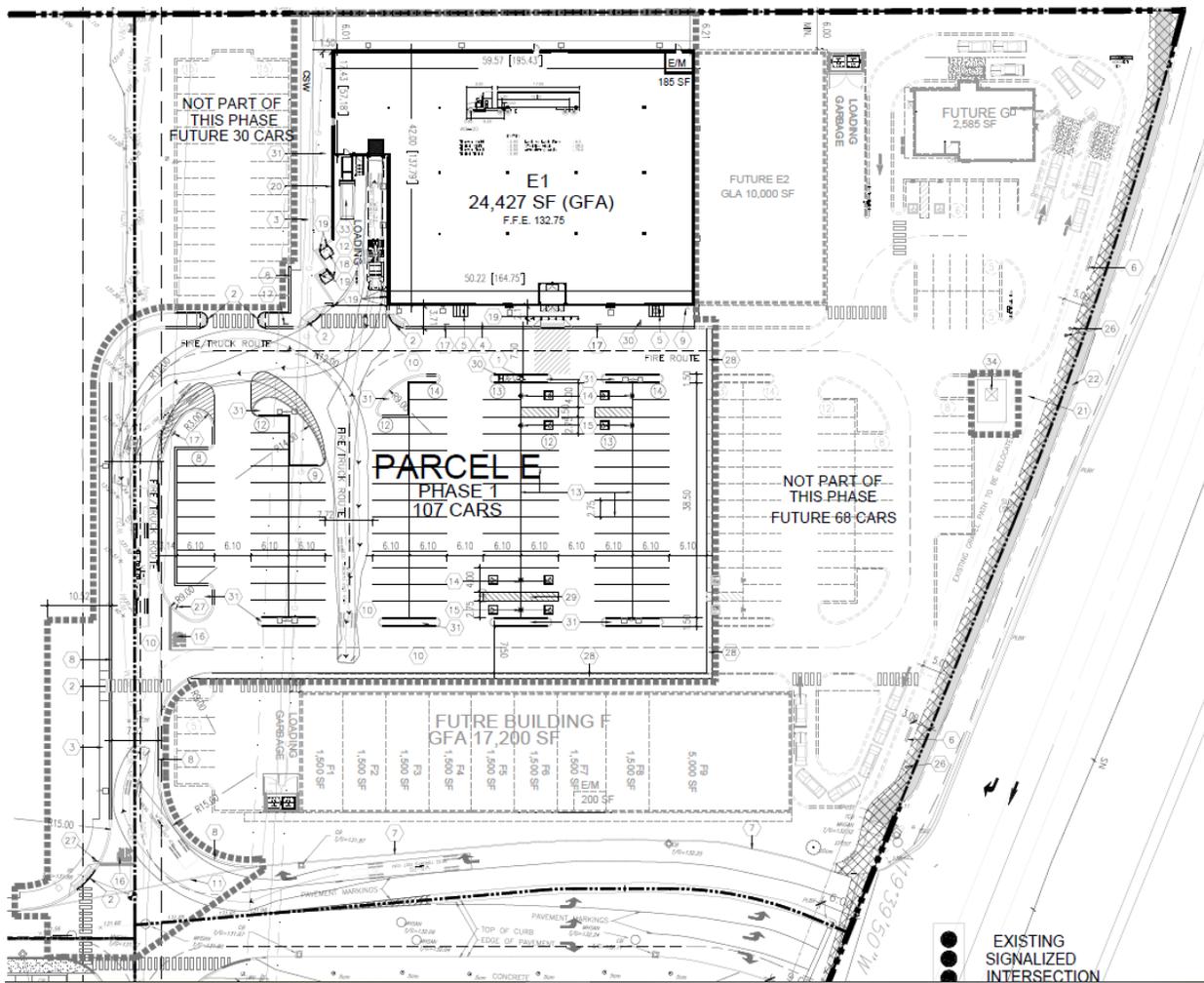


Figure 5. Topographic Sketch Certified December 7, 2023 (Stantec Geomatics Ltd.)



## 2 Proposed Development and Design Brief

The proposal represents the first phase of commercial retail development on a vacant portion of a shopping centre property (see site plan in Figure 6). The Phase 1 development will consist of a single commercial retail building (approximate gross floor area of 2,269.34 m<sup>2</sup>), a screened loading and waste storage area, surface parking (107 spaces), bicycle parking (8 spaces), drive aisles, and a mix of hard and soft landscaping (12 trees to be planted). The development will be on full municipal services and will have vehicle access to McNeely Avenue via the property's existing drive aisle along the south boundary of Parcel E. The development has been designed to conform with applicable planning policy and to meet the general intent of applicable regulations under the Town's Development Permit By-law, with amendments required for a minor reduction in the required parking rate for a retail store land use (114 spaces required and 107 spaces proposed) and for loading spaces (two spaces required and one proposed).



## Planning Rationale and Design Brief

Through follow-up consultation the Town was informed that the planned tenant for the building will require signage on the property to announce the project, and the Town confirmed that the approval of the signage, which will require relief from By-law 65-2008 (the Town's sign by-law), can be addressed through the DP3 process. The required relief for the two proposed signs is from Section 7.13.1 of the by-law, which restricts the size of a project announcement sign to an area of 3 m<sup>2</sup> whereas the planned tenant requires two signs, each of approximately 7.5 m<sup>2</sup> (see site plan identifying the location for the temporary project announcement signage as "General Notes" item 6). The signage request is to also allow for the signs to remain erected 60 days before and 30 days after the planned tenant's store opening (three months total).

As part of Phase 1 a north-south pathway along the westerly edge of the phase is proposed and will provide a connection between the existing shopping centre and the pathway network north of the property. Additionally, it is proposed that the existing granular MUP along the east frontage of Parcel E be re-aligned and conveyed to the County as a condition of development.

A Tree Inventory and Preservation Plan prepared by Dendron Forestry Services in support of the application noted that no Butternut or Hackberry trees were observed, and that within Parcel E a total of 19 trees with a diameter at breast height ("DBH") of 200 mm or greater are to be removed due to development conflict resulting from proposed building, parking, below grade electrical duct bank, and the need for clear sight lines for retail signage. Of these trees, a total of 13 are to be removed as part of Phase 1. For trees with a DBH of 200 mm or greater, the Town's Development Permit By-law requires a replacement ratio of one new tree for every three removed trees, which in the case of the Phase 1 development, would require 4.3 new trees, whereas a total of 6.3 new trees would be required for the full build out of Parcel E. Twelve trees for Phase 1 are to be planted in accordance with the Landscape Plan prepared by Levstek Consultants Inc. in support of the application, which is shown in Figure 7.

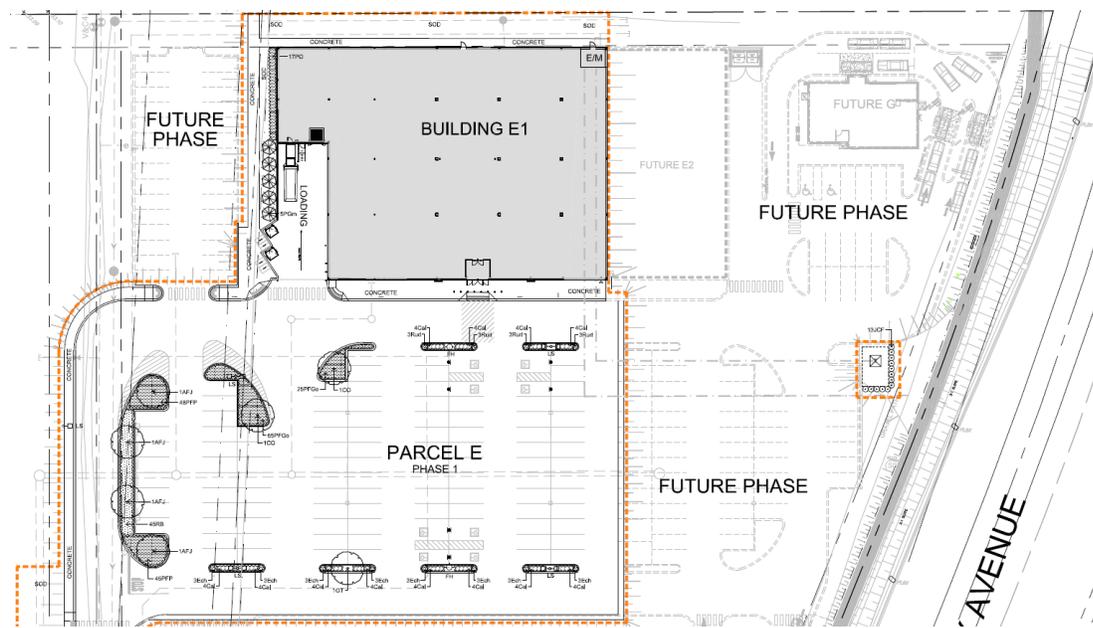


Figure 7. Landscape Plan Excerpt (Levstek Consultants Inc.)



## Planning Rationale and Design Brief

The building exterior design and material usage complement the design of the existing buildings at the shopping centre south of the subject site, and are influenced, in part, by the standard architectural requirements of the planned tenant. The building elevations and materials schedule are provided in figures 8 and 9 and show the building consisting of a mix of spandrel and transparent glazing, multi-coloured stucco, architectural blocks, and metal cladding along the front façade of the building (facing south), with the remaining facades to consist of stucco, architectural blocks, and metal cladding. The south (front), east (side), and north (rear) façades all have exposure to McNeely Avenue and will contain tenant signage. The east façade cladding is to be in place until such time that a future phase building gets constructed and converts this exterior wall to a party wall shared with a neighbouring tenant.

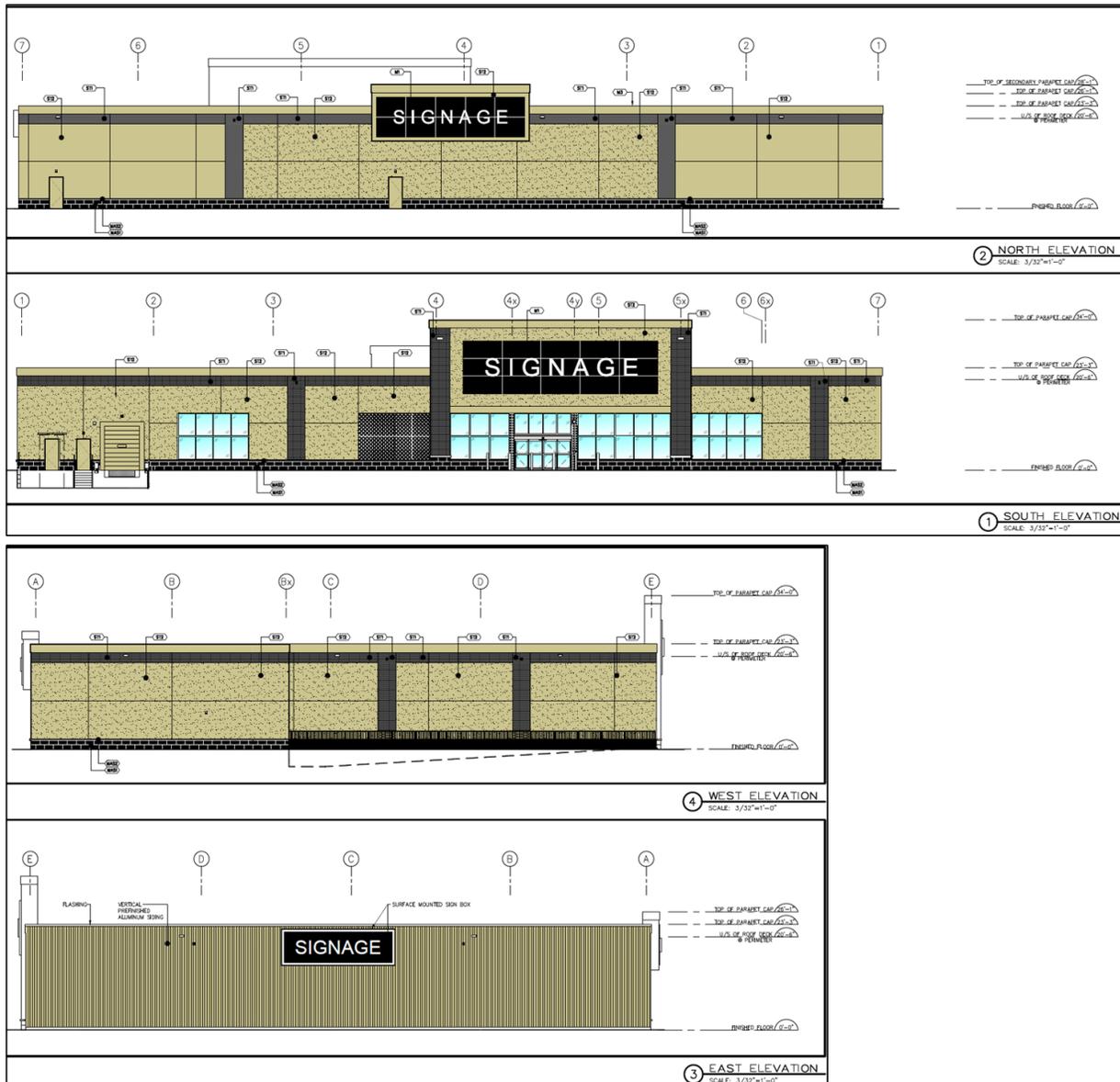


Figure 8. Building Elevations (LLA Architecture+)



## Planning Rationale and Design Brief

 <b>FINISH &amp; MATERIAL SCHEDULE</b>					
EXTERIOR FINISHES					
CODE	FINISH	COLOUR	PRODUCT DESCRIPTION	MANUFACTURER	LOCATION
ST1	STUCCO	WINN-06-1021S GREY (STRATO TONE) 	SAND PEBBLE TEXTURE- STRATO TONE ACHIEVES 6 YEAR FADE WARRANTY"	DRYVIT	EXTERIOR WALLS ACCENT
ST2	STUCCO	WINN-07-1021 BEIGE 	"SAND PEBBLE" FINISH - STRATO TONE ACHIEVES 6 YEAR FADE WARRANTY"	DRYVIT	EXTERIOR WALLS
MAS1	ARCHITECTURAL BLOCK	COLOUR: ONYX (FINISH: SUAVE) 	12"x24" FINESSE PROFILE	BRAMPTON BRICK	EXTERIOR WALL BASE
MAS2	ARCHITECTURAL SILL	COLOUR: ONYX (FINISH: SUAVE)	32" SILL	BRAMPTON BRICK	EXTERIOR MASONRY SILL
M1	METAL PANELS	REYNOBOND - DEEP BLACK ALPOLIC - BLACK	REYNOBOND - COLORWELD 500 SERIES OR ALPOLIC AS ALTERNATE MATERIAL	CLADCO (CLADDING SYSTEM C2000) OR AMNA (CLADDING SYSTEM AM2000)	EXTERIOR SIGNAGE
M3	METAL FLASHING	QC 56068	PRE - FINISHED METAL FLASHING	VICWEST	EXTERIOR FLASHING


**FINISH & MATERIAL SCHEDULE**  
 SCALE: 3/32"=1'-0"

Figure 9. Building Elevations - Finish and Material Schedule (LLA Architecture+)

In accordance with the Planning Act and By-law 103-2019 (Town's Parkland Dedication By-law) parkland dedication is to be satisfied as cash-in-lieu of parkland at the commercial rate of 2% of the value of the Phase 1 land area. The owner is to submit a market appraisal of the Phase 1 lands to the Town for review, and if satisfied, then the Town will identify the 2% value to be paid as cash-in-lieu of parkland prior to the issuance of a building permit.

The balance of Parcel E, which is not proposed for development as part of the subject Phase 1 DP3 application, is conceptually envisioned as additional commercial development consisting of the following:

- Building E2, being a single tenant building abutting the proposed Phase 1 building
- Building F, being a multi-tenant building containing a single drive-through
- Building G, being a standalone, restaurant related drive-through
- Approximately 2,767 m<sup>2</sup> of gross floor area
- Approximately 68 vehicle parking spaces
- Associated drive aisles, drive-through queuing, and landscaping



### 3 Policy Review and Justification

#### 3.1 Provincial Policy Statement 2020

The Provincial Policy Statement 2020 (“PPS”) provides policy direction on planning matters in the Province of Ontario and is issued under Section 3 of the Planning Act. This direction can be generally described as being for the planning of strong, sustainable, and resilient communities for all people, for clean and healthy environments, and for strong and competitive economies. Decisions affecting planning matters shall be consistent with the policies of the PPS.

The below review demonstrates that the proposed development is consistent with the applicable policies of the PPS.

##### **Section 1.0 Building Strong Healthy Communities**

Subsection 1.1.1 sets out criteria whereby healthy, livable, and safe communities are sustained, which include such things as: promoting efficient development and land use patterns; accommodating an appropriate affordable and market-based range and mix of employment and other uses to meet long-term needs; avoiding development and land use patterns which may cause environmental or public health and safety concerns; improving accessibility by identifying, preventing, and removing land use barriers; promoting cost-effective development patterns; ensuring there is necessary infrastructure to support growth; promoting development and land use patterns that conserve biodiversity; and, preparing for the regional and local impacts of a changing climate.

Subsection 1.1.3.2 states, in part: land use patterns within settlement areas shall be based on densities and a mix of land uses which efficiently use land and resources and support active transportation.

Subsection 1.3.1 states, in part: planning authorities shall promote economic development and competitiveness by: providing for an appropriate mix and range of employment [...] uses to meet long-term needs; providing opportunities for a diversified economic base [...]; and, facilitating the conditions for economic investment by identifying strategic sites for investment [...].

Subsection 1.6.6.2 states: municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.

Subsection 1.6.6.2 states, in part, that planning for stormwater management shall be: integrated with planning for sewage and water services and ensure that systems are optimized, feasible and financially viable over the long term; minimize erosion and increases in contaminant loads; mitigate risks to human health, safety, property, and the environment; maximize the extent and function of vegetative and pervious surfaces; and, promote stormwater management best practices.



## Planning Rationale and Design Brief

Subsection 1.7.1 states, in part, that long-term economic prosperity should be supported by promoting opportunities for economic development and community investment-readiness.

The proposed commercial development is consistent with and supportive of the above policy statements of Section 1.0, as it will:

- provide an appropriate and desirable infill of land designated for commercial use;
- support active transportation with the introduction of a new north-south pedestrian pathway connecting the shopping centre to the residential neighbourhood and path network north of the site, as well as re-alignment and paving of an existing temporary MUP along the site's frontage;
- promote economic development and the efficient use of land and infrastructure by preparing the site for proposed and future development;
- introduce service and retail type employment opportunities to support the Town's growing population; and,
- optimize municipal services and provide soft landscaping and tree planting.

### Section 2.0 Wise Use and Management of Resources

Section 2 of the PPS contains policy statements regarding the wise use, management, and protection of significant resources.

In accordance with supporting plans and studies, the proposal is not anticipated to result in any impacts on matters relating to resources under Section 2 of the PPS. The Town's list of required plans and studies to support the proposal did not include an Environmental Impact Statement, and the prepared Tree Inventory and Preservation Plan did not identify any Butternut or Hackberry trees. Other relevant studies and plans, such as a geotechnical study, an erosion and sediment control plan, and a site servicing and stormwater management report were also provided in support of the application.

### Section 3.0 Protecting Public Health and Safety

Section 3 of the PPS contains policy statements that direct development away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.

In accordance with supporting plans and studies, the proposal is not anticipated to result in any unacceptable risk to public health and safety. The required submissions included a site servicing and stormwater management report that considered potential storm events per the criteria established by the Town.

The proposal will introduce a desirable commercial retail development on land designated to accommodate it, which among other benefits, will support economic vitality and the efficient use of land



and public infrastructure. For these reasons and others noted in the above review, the proposal is consistent with the policies of the PPS 2020.

### 3.2 Lanark County Sustainable Communities Official Plan (2012)

Adopted in 2012, the Lanark County Sustainable Communities Official Plan (“SCOP”) is an upper-tier policy document approved under the Planning Act that provides guidance for land use planning within the County of Lanark. The SCOP primarily serves a regional purpose with the intent that greater policy direction on local matters is to be provided by local Official Plans, such as the Town of Carleton Place Official Plan adopted in 2013. promotes the integration of sustainable practices into land use planning across Lanark County. Land use planning decisions shall demonstrate conformity with the applicable policies of the SCOP.

Schedule A of the SCOP designates the Town of Carleton Place as a Settlement Area and McNeely Avenue as an Urban Arterial road. Subsection 2.6 of the SCOP contains Settlement Area land use policies, with Subsection 2.6.1 stating the following with respect to objectives for this designation:

#### 2.6.1 Objectives

The County of Lanark’s objectives respecting development in Settlement Areas are as follows:

5. To provide for mixed use communities with appropriate commercial, institutional and employment uses.

The proposed development conforms to the intent of the Settlement Area designation as it will introduce a commercial land use to an underutilized shopping centre site along a County Road within the Town of Carleton Place.

### 3.3 Town of Carleton Place Official Plan (2013)

The Town of Carleton Place Official Plan (“OP”) provides the policy framework which will guide land use decisions within the Town’s boundaries for the next 20 years. The OP was approved in 2013 and last consolidated in 2021, with a recent amendment approved by Town Council as By-law No. 63-2023 (currently under review by the County of Lanark). The following review has consideration for the relevant policies that have been modified as part of By-law No. 63-2023 which are not yet in full force and effect.

Schedule A of the OP designates the subject site as Highway District (see Figure 10). Subsection 3.3 of the OP contains land use policies for the Highway District designation, with a review of relevant intent and policies provided below.



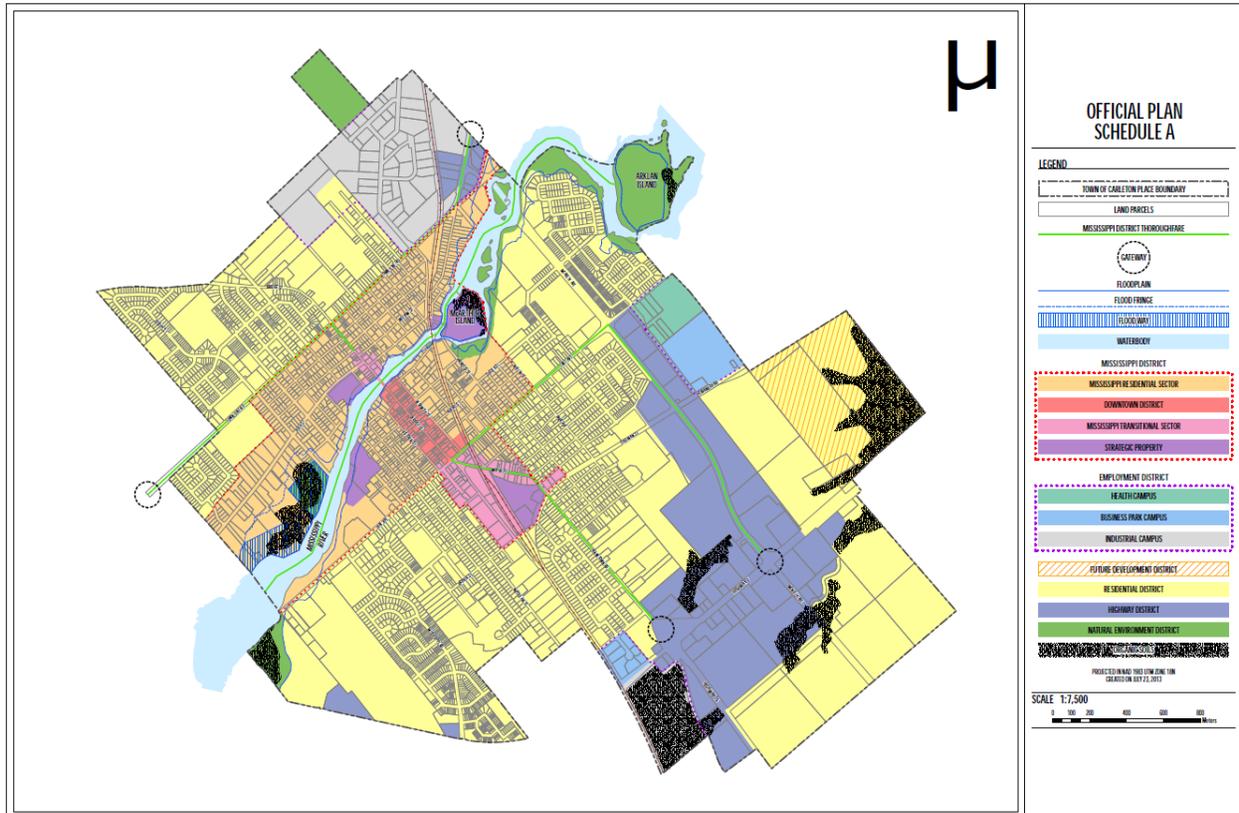


Figure 10. Town of Carleton Place Official Plan Schedule A

3.3 [...] The Highway District designation is intended to guide the on-going development of regional scale commercial retail facilities located along Highway 7 and McNeely Avenue.

### 3.3.2. Permitted Uses

3.3.2.3 Permitted uses include a full range of retail commercial uses, including anchor stores such as supermarkets, department stores, apparel, home furnishings and building materials, automotive uses, drug and cosmetic, retail and service commercial uses directed to the traveling public, movie theatres and other entertainment type uses as well as hotels and motels.

The proposed commercial retail use is permitted.

### 3.3.3. Policies

3.3.3.1 All development in the Highway Commercial District shall be subject to a pre-development consultation process prior to submission of any required application. Pre-development consultation shall be based on a preliminary site development concept plan prepared to a sufficient level of detail to assess traffic patterns, access, parking and loading facilities, location and massing of buildings, adjacent land uses, potential impacts and site aesthetics. Any additional information required to ensure a complete



## Planning Rationale and Design Brief

application in accordance with the relevant policies of Section 6.0 - Implementation will be established during the pre-development consultation.

3.3.3.2. All new development in the Highway Commercial District shall be supported by technical reports prepared by qualified professionals. Such reports may include a traffic study, serviceability report and a stormwater management report as required. The pre-consultation process shall determine the need for these studies or any additional studies and reports.

3.3.3.4. A Class 3 Development Permit process shall apply to all Regional Commercial development as well as any redevelopment which would have the effect of increasing parking and/or loading requirements or which substantially changes the scale and or density of the existing development.

A formal pre-development consultation meeting with the Town took place on October 5, 2023. Through that meeting and additional correspondence with the Town it was confirmed that a Class 3 Development Permit application and specific technical plans and reports would be required to support the proposed development.

3.3.3.3. A high standard of building and landscape design shall be applied through the Development Permit By-law particularly where development is proposed adjacent to residential uses or is located in a strategic location.

The proposal is on lands that abut existing shopping centre development to the south, McNeely Avenue to the east, vacant shopping centre lands to the west, and a stormwater management pond and pedestrian pathway to the north. The proposed building and landscape design achieve the Built Form Design Criteria for the Highway District, as demonstrated later in this subsection of the report.

Section 2 Community Design Framework of the OP contains general design direction for development within the Town, with a review of relevant intent and policies provided below.

2.3.1 Proposed developments shall enhance the image of the Town of Carleton Place by complementing and contributing to:

- the consistency and continuity of the area with its surroundings;
- linkages within, to and from the area.

2.3.6 The design of new development shall:

- be complementary to adjacent development in terms of its overall massing, orientation and setback;
- provide links with pedestrian, cycling and road networks;
- enhance orientation and integrate newly developing areas of the Town of Carleton Place.

2.3.7 Development or redevelopment design shall strive to achieve the following:

- provide a development pattern that supports a range of uses;



## Planning Rationale and Design Brief

- provide transportation connections, including pedestrian and cycling connections to adjacent areas.

2.3.10 The Town shall promote and encourage building facades to be visually interesting through extensive use of street level entrances and windows. Functions that do not directly serve the public, such as loading bays and blank walls, should not be located directly facing the street.

2.5 Energy efficiency and conservation will be encouraged by approving developments that:

- provide for pedestrian and bike path facilities;

- have a compact pattern of development that clusters compatible uses within close proximity to one another.

The proposal is for a first phase of commercial development which will achieve the following:

- Land use and built form compatibility and consistency with adjacent shopping centre development;
- Enhancement of the mobility network by providing a new north-south pedestrian pathway through the site that will connect to the City pedestrian network to the north, by providing on-site bicycle parking, and by proposing the re-alignment and paving of the existing MUP along the site's frontage;
- Opportunity for a mix of commercial type land uses as part of future phases; and,
- Site design that screens the loading area of the proposed building from the adjacent public street and that promotes storefront visibility.

Section 4 Municipal Amenities and Green Infrastructure of the OP contains policies on such things as open space, parks, natural heritage and infrastructure that help to support a healthy and pleasant environment, with a review of relevant intent and policies provided below.

### 4.1 Green Infrastructure

#### 4.1.6 Street Trees

1. Street trees and the preservation of tree canopies shall be protected to the greatest extent possible. Accordingly a tree preservation plan may be required in support of development applications.

4. Tree planting and tree preservation will occur so that all areas of the Town are provided with a sufficient number of trees to maintain a high standard of amenity and appearance. Where new development will result in the loss of existing wooded areas, a condition of development approval will require that the lost trees be replaced at a 1 to 3 ratio (1 new tree for every 3 trees). For the purposes of this policy the replacement ratios will only apply to the removal of trees having a minimum caliper of 200mm or more. The new trees will be planted within the boundary of the proposed development to the greatest extent possible with the remaining trees to be planted in public parks or on publicly owned lands as directed by the Town. The caliper size and tree species shall be a condition of the development



## Planning Rationale and Design Brief

approval. The requirements of this policy shall be in addition to any other landscaping requirements associated to any particular development application.

A Tree Inventory and Preservation Plan prepared by Dendron Forestry Services in support of the application noted that no Butternut or Hackberry trees were observed, and that within the Phase E lands a total of 19 trees with a diameter at breast height (“DBH”) of 200 mm or greater are to be removed due to development conflict resulting from the proposed building, parking, below grade electrical duct bank, and the need for clear sight lines for retail signage. Of these trees, a total of 13 are to be removed as part of the Phase 1 development. For trees with a DBH of 200 mm or greater, the Town’s Development Permit By-law requires a replacement ratio of one new tree for every three removed trees, which in the case of the Phase 1 development, would require 4.3 new trees, whereas a total of 6.3 new trees would be required for the full build out of Parcel E. Twelve trees for Phase 1 are to be planted in accordance with the Landscape Plan prepared by Levstek Consultants Inc. in support of the application, which is shown in Figure 7.

### 4.2 Parks and Open Space System

#### 4.2.3. General Policies

3. New development may be required to incorporate an integrated recreational walkway / trail system, interconnecting residential neighbourhoods, commercial areas, employment areas, schools, public buildings, and major recreation facilities.

11. The Town may require wider road right-of-ways on roads to be located in new development areas in order to accommodate a pedestrian / cycling path within the road right-of-way. In such cases the Town will apply density bonusing in accordance with section 3.5.4.1 Section 3.5.4 and 3.5.5.

### 4.3 Built Infrastructure

#### 4.3.3 Transportation

##### 4.3.3.7 Land Acquisition

Land may be acquired by the Town for road widenings, road extensions, rights of way, or intersection improvements. Such land may be acquired through the subdivision or consent process, through Development Permit conditions or through formal agreements. The minimum rights-of-ways for highways and roads are shown on Schedule A.

As part of Phase 1 a north-south pathway along the westerly edge of the phase is proposed and will provide a connection between the existing shopping centre and the pathway network north of the property.

Schedule B identifies McNeely Avenue as an Arterial Road with a local trail. It is understood that the “local trail” represents the existing temporary MUP which crosses through the frontage of the owner’s land and consists of granular where adjacent to the subject site. Through pre-consultation correspondence



## Planning Rationale and Design Brief

with the Town and County it was confirmed that the MUP is to be re-alignment, paved, and conveyed to the County as a condition of development.

### 4.3 Built Infrastructure

#### 4.3.5 Water, Waste Water and Stormwater Services

##### 4.3.5.1 General Policies

1. All development shall generally occur on the basis of full municipal water and wastewater services.
5. The allocation of infrastructure capacity for infill and economic development purposes is encouraged.
6. Stormwater management will be required for all new development in accordance with guidelines which may be developed by the Ministry of the Environment, the Mississippi Valley Conservation or the Town of Carleton Place. [...]

The proposed development will be on full municipal services, will not require the expansion of public services, and is supported by a site servicing report provided with the DP3 application.

The proposed development conforms to the applicable policies of the OP.

## 4 Development Permit By-law 15-2015, as amended

The Town's Development Permit By-law ("DPB") serves to regulate land use and development within the Town. The following section provides a review of the applicable regulations of the DPB and includes rationale to support the minor amendments required from the minimum parking requirement for a retail store land use (114 spaces required and 107 spaces proposed) and for loading spaces (two spaces required and one proposed).

Development Permit Schedule A identifies the subject site as being within the Highway District (see Figure 11), with Section 8 of the DPB containing regulations specific to this district and stating the following: The Highway District designation is intended to guide the on-going development of regional scale commercial retail facilities located along Highway 7, McNeely Avenue, and Captain A. Roy Brown. The below table and subsequent review demonstrate how the proposed development complies with the provisions of the DPB, save and except the two aspects subject to the requested amendments.



# Planning Rationale and Design Brief

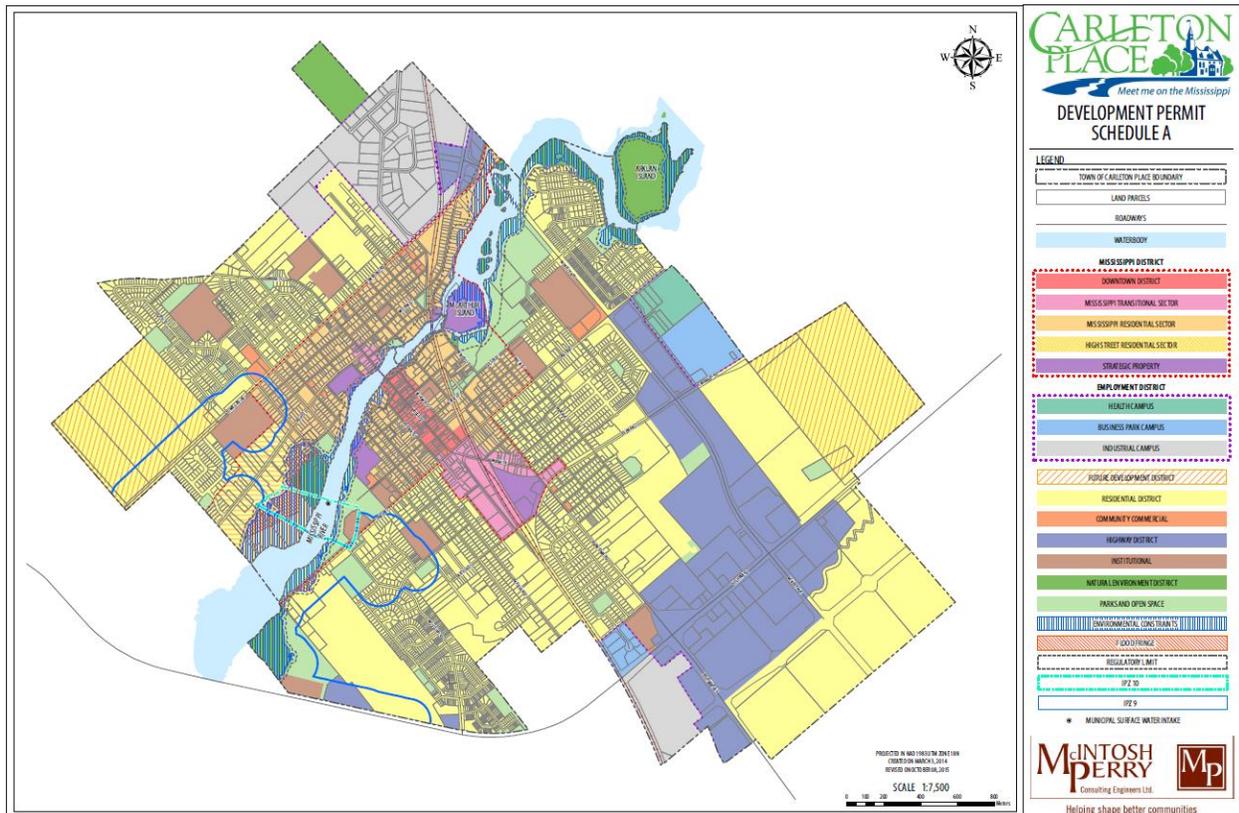


Figure 11. Town of Carleton Place Development Permit Schedule A

Table 1. Development Standards Compliance Review

DPB Section	Provision / Development Standard	Required / Permitted	Proposed	
8.1	Permitted Uses	Retail Store	Retail Store	
8.3	Lot Area (minimum)	Nil	±118,874 m <sup>2</sup> (property); ±21,591 m <sup>2</sup> (Parcel E); ±10,640 m <sup>2</sup> (Phase 1)	
	Lot Coverage (maximum)	60%	±21% (Phase 1)	
	Lot Frontage (minimum)	15.0 m (49.2 ft)	±152 m (Parcel E / Phase 1)	
	Front Yard	6.0 m (19.6 ft)	±61 m	
	Exterior Side Yard	6.0 m (19.6 ft)	NA	
	Interior Side Yard (minimum)	3.0 m (6.5 ft) where lot abuts residential use		NA
		1.2 m (3.9 ft) where lot abuts any other designation		6.01 m
Rear Yard Depth (minimum)	6.0 m (29.5 ft)		±240.57 m	



## Planning Rationale and Design Brief

	Building Height (maximum)	11.0 m (36 ft)	±6.2 m (top of roof deck) – see building elevation drawing prepared by LLA Architecture+
	Landscape Strip	3.0 m (9.8 ft)	North Interior Side Yard: 6.01 m;  Remaining yards: NA for Phase 1 as the phase area excludes the owner's lands abutting other lot lines. Adequate space is available to meet the requirement during future phases, and if development plans at that time result in conflict with the required standard, then an amendment to the standard will be considered.
3.29.3	Number of Parking Spaces for a Retail Store	1 space per 20 m <sup>2</sup> (215 ft <sup>2</sup> ) of gross floor area, minimum 5 spaces  = 2,269.34 m <sup>2</sup> / 20 = 114	107 [amendment required]
3.29.2	Number of Barrier-free Parking Spaces	Capacity of public parking area: 100-199 = 2	8
3.29.1), 3.29.2.1)	Parking Space Dimensions	Standard Parking Space: 6 m long by 2.75 m wide	6.1 m long by 2.75 m wide
		Barrier-free Parking Space: 6 m long by 3.7 m wide	6.1 m long by a minimum of 4 m wide
		Barrier-free Parking Space Width for Type A and Type B: 3.4 m space, 1.5 m aisle, 2.4 m space	4 m space, 1.5 m aisle, 2.75 m space
3.29.4)c.	Parking Space Access and Egress	shall be by means of a driveway, land, or aisle at least 3.5 m for one way traffic and 6 m for two-way traffic	Driveway is a minimum width of 7.5 m and drive aisles are 6.1 m
3.25.1	Number of Loading Spaces	2 spaces for a building with a floor area exceeding 1,000 m <sup>2</sup> but not more than 7,500 m <sup>2</sup>	1 [amendment required]
3.25	Loading Space Dimensions	9 m long by 3.5 m wide, and with a vertical clearance of 4.5 m	±24 m long by ±4 m wide, with a vertical clearance of 4.5 m
	Loading Space Access and Egress	shall be by means of a driveway at least 3.5 m for one way traffic and 6 m for two-way traffic	Driveway and drive aisle providing access to the loading space is a minimum width of 7.5 m
3.31	Number of Bicycle Parking Spaces for Commercial Uses	5% of required vehicle parking; or 8 spaces, whichever is greater = 6	8



## Planning Rationale and Design Brief

3.31.1	Bicycle Space Design Standards	1.8 m long by 0.6 m wide, with a 1.2 m clear access aisle and 1.2 m vertical clearance	1.8 m long by 0.6 m wide, with a 3.3 m clear access aisle and 1.2 m vertical clearance
8.4.2	Landscaping shall be provided along all front, side and rear yards.		North Interior Side Yard: 6.01 m;  Remaining yards: NA for Phase 1 as the phase area excludes the owner's lands abutting other lot lines. Adequate space is available to meet the requirement during future phases, and if development plans at that time result in conflict with the required standard, then an amendment to the standard will be considered.
	Five metres (16.4 feet) of landscaped open space shall be provided along all arterial and collector roadways		NA for Phase 1 as the phase area excludes the lands nearest the arterial roadway.  Adequate space is available to meet the requirement during future phases, and if development plans at that time result in conflict with the required standard, then an amendment to the standard will be considered.
	1.5 metres (4.9 feet) of landscaped open space shall be provided between abutting and similar land uses.		How this standard applies is unclear and to be confirmed by the Town through review of the DP3 application in support of Phase 1.

### Subsection 3.15 Exterior Design

Elements of exterior design are subject to the provisions outlined in Sections 13 and 14 of this By-law. All proposals must include coloured elevation drawings which demonstrate that the overall design of the new development is consistent with the exterior design elements illustrated in Sections 13 and 14 of this By-law.

Colour elevations for the single proposed building of the Phase 1 development have been provided in support of the DP3 application. As discussed with Town staff through pre-application consultation, the east elevation of the Phase 1 building will serve a temporary purpose, as that portion of the building will eventually be removed as part of a future phase to accommodate a building addition creating space for a second tenant.

Section 14.5 of the DPB contains built form design direction for development within the Highway District. The below table lists the relevant design criteria for commercial retail development along with an explanation on how the proposal has consideration for the criteria.



## Planning Rationale and Design Brief

Table 2. Review of the Highway Commercial Built Form Design Criteria

Design Criteria	Design Response
<p>Buildings should be oriented towards the street and parking provided in the rear or side of building. Where property fabric will not lend itself to the provision of parking except at the front of the building, parking will be buffered and screened by landscape materials providing an element of all season screening. At no time will parking be provided within the front yard setback.</p> <p>Buildings located at intersections should provide exterior details on both street fronts. These areas will provide gateways into the commercial node.</p>	<p>The proposed development is on lands within a shopping centre and represents a first phase on vacant lands intended to be built out with additional buildings. The building is not accessed from its own driveway off of McNeely Avenue, and instead, is accessed from the existing drive aisle serving the entire shopping centre. The building is oriented towards this internal drive aisle (facing south) which allows for its back-of-house operations to be located along the interior and rear yard of the property and visually screened from McNeely Avenue. Signage and cladding proposed along the building's east elevation is consistent with the architectural expression of the existing shopping centre and the requirements of the planned tenant.</p> <p>Proposed parking is appropriately centralized within the Parcel E area, with the intent to have buildings situated along the perimeter, and for the majority of the frontage to exclude buildings and to consist of landscaping to maintain clear sightlines throughout the shopping centre for commercial exposure to the travelling public.</p> <p>The Phase 1 area does not include the area to contain the required front yard setback, and so no parking is proposed within the required front yard setback.</p>
<p>Where a building elevation is adjacent to a roadway, elevations will be required to have an appropriate number of projections and recessed areas and a variety of cladding types in order to ensure that a monotonous building façade is not created.</p> <p>All Highway District buildings shall have a minimum of 20% glazing on their front façade to create a human scale environment.</p>	<p>Signage and cladding proposed along the building's east elevation is consistent with the architectural expression of the existing shopping centre and the requirements of the planned tenant. As part of a future phase, it is envisioned that a new building will extend from the east facing wall of the proposed building.</p> <p>Given the above, which has been discussed with Town staff through pre-application consultation, no glazing has been added to the east elevation. Approximately 25% of glazing (transparent and spandrel) is provided on the building's primary façade facing south.</p>
<p>Where a highway commercial use is located adjacent to a residential use a 3.0 metre (9.8 feet) landscaped buffer strip shall be provided.</p>	<p>The proposed building is adjacent to a stormwater management pond on lands identified as Highway District in the DPB. Nonetheless, a landscaped buffer strip of approximately 6.01 metres is provided along this adjacency.</p>



## Planning Rationale and Design Brief

<p>A minimum of 5.0 metres (16.4 feet) of buffered landscaped area providing an element of all season screening shall be provided within the 6.0 metre (19.6 feet) front yard setback.</p>	<p>NA - the Phase 1 area does not include the area to contain the required front yard setback. The area for the required front yard setback would be included with a future phase of development on Parcel E.</p>
<p>All building service areas shall be located away from public view.</p>	<p>The building service area is located along the west of the building, which is out of sight from McNeely Avenue. Waste storage will be located within the proposed loading area, which will be screened from McNeely Avenue by building placement.</p>
<p>Pedestrian crosswalks and pathways either internal to the site or providing pedestrian access to the site will be defined with the addition of unit pavers or feature pavers.</p>	<p>Depressed curbs with tactile warning surfaces are proposed where pedestrian walkways cross drive aisles. The crossing will be painted with appropriate linework to indicate the walkway to users and vehicles. The use of paint over pavers in these areas will help to avoid conflicts with snowplow buckets which can result in ongoing maintenance and liability issues.</p>
<p>Entrances to parking areas should be well defined by signage and curbing. Wherever possible abutting commercial parking lots should be linked to provide for consolidated parking and ease of vehicular movements. Parking lots shall provide landscape elements within the islands and along thoroughfares. Provision for pedestrians within the site must be demonstrated. Cyclists must also be considered when developing site layout.</p>	<p>The proposed parking has been intentionally consolidated and centralized for Parcel E to simplify circulation and parking movements. Soft landscaping and tree planting has been incorporated throughout the parking lot in landscape islands and perimeter spaces (12 trees proposed). Pedestrian connectivity through the site is supported with appropriate walkways and crosswalks, and bicycle parking is provided in two accessible and well-lit areas flanking the main entrance to the building.</p>
<p>Buffers shall consist of plant material screens and structural elements that are designed and maintained in a staggered or undulating manner to create a more natural looking landscape. Not less than 50% of the landscaped area provided shall be natural earth comprised of the natural planting of grass, lawns, trees, shrubs and flowers in such a manner as to establish and enhance the beautification of the landscaped area and any building or structure on the same lot. Soft landscape materials may also include shrubs and planting at foundation base, container planting consisting of urns, planters, window boxes or suspended baskets.</p> <p>Plant material should be selected with regard for the annual climate of Carleton Place and the ability to tolerate salt. Native species are preferred.</p>	<p>The proposed landscape plan provides for softscape, where feasible, and additional landscaping will be provided through future phases until all of Parcel E is built-out.</p>
<p>Buildings should be integrated with the existing grade and provide at grade entrances or alternatively accessible modifications should be provided.</p>	<p>Building access, grades, and site circulation have been designed with consideration for accessibility.</p>



## Planning Rationale and Design Brief

<p>Garbage enclosures are to be fenced with wood screen and buffered with soft landscape elements.</p> <p>Garbage receptacles require screening (wood or ornamental metal facing). Recommended locations include inside parking courts or at the end of parking bays. Locations should be conveniently accessible for trash collection and maintenance and should not block access drives during loading operations.</p>	<p>Waste storage will be located within the proposed loading area, which will be screened from McNeely Avenue by building placement.</p>
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### 4.1 Amendment Rationale

Through review of the provisions and development standards of the DPB that apply to the Phase 1 development, it was determined that relief from the DPB would be required in two areas relating to parking, which are listed below along with rationale for each.

Table 3. Rationale for Proposed Amendments to the DPB

DPB Requirement	Proposed	Rationale
<p>3.25.1</p> <p>Number of loading spaces: 2 spaces for a building with a floor area exceeding 1,000 m<sup>2</sup> but not more than 7,500 m<sup>2</sup></p>	<p>1</p>	<p>The proposed loading space and loading area is designed in accordance with the tenant's specific needs. The proposed building includes a single loading bay and so only one loading space is needed for the tenant's operations. A single loading bay and space is common for commercial retail stores of this size and context, as demonstrated elsewhere within the subject shopping centre and with similar sized retail buildings located in other shopping centres further south along McNeely Avenue. Accordingly, the proposed reduction from two to one loading space is appropriate.</p>
<p>3.29.3</p> <p>Number of Parking Spaces for a Retail Store: 1 space per 20 m<sup>2</sup> (215 ft<sup>2</sup>) of gross floor area, minimum 5 spaces</p> <p>= 2,269.34 m<sup>2</sup> / 20 = 114</p>	<p>107</p>	<p>The proposed amount of parking substantially exceeds the tenant's specific needs, with additional spaces as contingency. The tenant is understood to require 3.4 spaces per 1,000 ft<sup>2</sup> of gross floor area, which results in 84 total spaces, whereas the Phase 1 proposal includes 107 spaces.</p> <p>The difference of seven spaces (6.1% delta) between what is being provided and what is required is negligible and is not anticipated to result in any parking related concerns.</p>

## 5 Supporting Documentation

A formal pre-application consultation with the Town was held on October 5, 2023, where the proposal was presented, and the Town provided initial feedback. Following the pre-application consultation, the



## Planning Rationale and Design Brief

Town confirmed the application type and supporting documentation required in order to support the proposed development through the approvals process. The below listed technical documents, as identified by the Town, were prepared in support of the proposed Phase 1 development and required DP3 application.

1. Site Plan – Petroff Partnership Architects
2. Landscape Plan – Levstek Consultants Inc.
3. Tree Inventory and Preservation Plan – Dendron Forestry Services
4. Planning Rationale and Design Brief – Stantec Consulting Ltd.
5. Coloured Building Elevations – LLA Architecture+
6. Servicing and Stormwater Management Report – Counterpoint Engineering Inc.
7. Servicing Plan – Counterpoint Engineering Inc.
8. Grading and Drainage Plan – Counterpoint Engineering Inc.

## 6 Conclusion

This planning rationale and design brief has been prepared in support of a Class 3 Development Permit application for a proposed commercial development on a vacant portion of the property described municipally as 450 McNeely Avenue in the Town of Carleton Place. The proposed development represents a first phase of development on a vacant portion of the subject property that is described as Parcel E. The Phase 1 development generally consists of a single commercial retail building, parking, landscaping, and a north-south pedestrian pathway. The DP3 application will serve the following purposes:

- Request approval for the proposed Phase 1 development, including the two minor aspects of relief required from the DPB;
- Request approval of two project announcement signs, each approximately 7.5 m<sup>2</sup> in size and to be erected for a period of three months (60 days before and 30 days after the planned tenant's store opening); and,
- Detail the re-alignment of the MUP to be partially located within the subject property.

As demonstrated through this report and in the technical material required with the application, the proposed Phase 1 development represents an appropriate use of vacant shopping centre land, as it will help to infill the property with a land use and site design that aligns with the intent of applicable planning policy and regulations. Accordingly, we recommend the application for approval.

